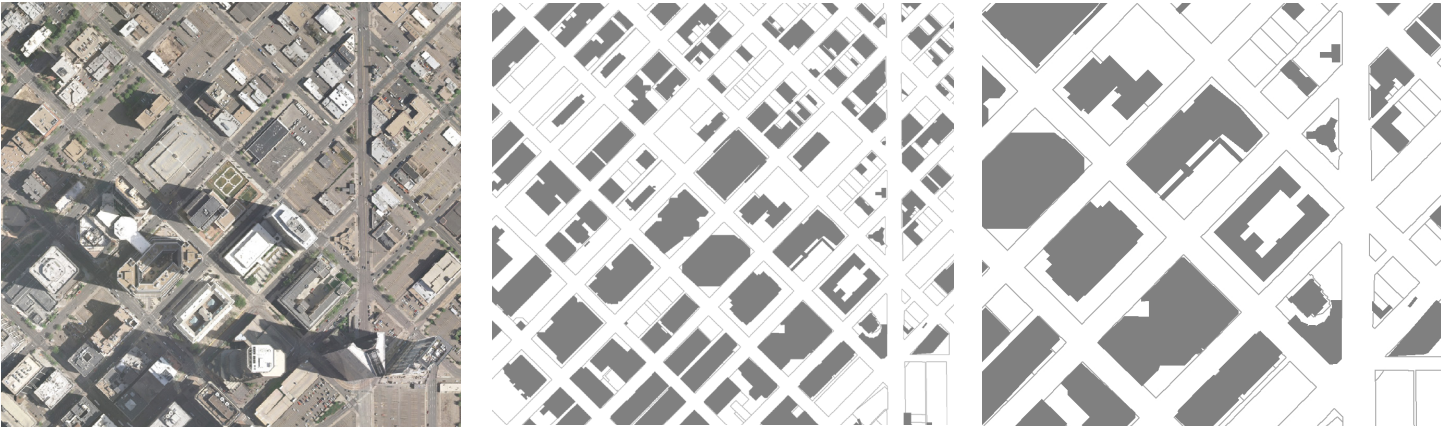


PUD-G 19



1901 Wazee Street

2017I-00156

November 13, 2018

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PUD-G 19

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CHAPTER 1. ESTABLISHMENT AND INTENT

SECTION 1.1 PUD-G 19 ESTABLISHED

The provisions of this PUD-G 19 apply to the land depicted on the Official Zoning Map with the label PUD-G 19, and legally described as:

A PARCEL OF LAND SITUATED IN A PORTION OF BLOCK C, EAST DENVER, AND BLOCK 1, HOYT & ROBINSONS ADDITION TO DENVER, BEING A PORTION OF THE SOUTHWEST ONE-QUARTER OF SECTION 27, TOWNSHIP 3 SOUTH, RANGE 68 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY AND COUNTY OF DENVER, STATE OF COLORADO MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID BLOCK C, EAST DENVER, SAID POINT ALSO BEING AT THE NORTHEASTERLY RIGHT OF WAY INTERSECTION OF 19TH STREET AND WAZEE STREET, SAID POINT BEING THE TRUE POINT OF BEGINNING; THENCE NORTH 45 DEGREES 26 MINUTES 17 SECONDS WEST ALONG THE SOUTHWESTERLY LINE OF SAID BLOCK C, EAST DENVER AND EXTENSION THEREOF AND THE NORTHEASTERLY RIGHT OF WAY LINE OF SAID 19TH STREET A DISTANCE OF 326.09 FEET TO A POINT ON THE NORTHEASTERLY EXTENSION OF THE WYNKOOP STREET 20.00 FOOT RANGE LINE; THENCE NORTH 45 DEGREES 26 MINUTES 37 SECONDS WEST ALONG THE SOUTHWESTERLY LINE OF SAID BLOCK 1, HOYT & ROBINSONS ADDITION TO DENVER AND EXTENSION THEREOF AND THE NORTHEASTERLY RIGHT OF WAY LINE OF SAID 19TH STREET A DISTANCE OF 19.40 FEET TO THE SOUTHEASTERLY CORNER OF A PARCEL OF LAND CONVEYED TO THERMO HEAD HOUSE, L.L.C. BY SPECIAL WARRANTY DEED RECORDED OCTOBER 13, 1994 AT RECEPTION NO. 9400156352; THENCE NORTH 44 DEGREES 21 MINUTES 17 SECONDS EAST ALONG SAID SOUTHEASTERLY LINE OF RECEPTION NO. 9400156352, A DISTANCE OF 375.95 FEET TO A POINT ON THE SOUTHWESTERLY LINE OF ACQUISITION PARCEL TK 2279-00-005 REV. 1, CONVEYED TO THE CITY AND COUNTY OF DENVER BY SPECIAL WARRANTY DEED RECORDED APRIL 07, 1993 AT RECEPTION NO. R-93-0043076 IN THE CITY AND COUNTY OF DENVER CLERK AND RECORDER'S OFFICE; THENCE SOUTH 46 DEGREES 55 MINUTES 34 SECONDS EAST ALONG SAID SOUTHWESTERLY LINE OF RECEPTION NO. R-93-0043076, A DISTANCE OF 281.08 FEET; THENCE CONTINUING ALONG SAID SOUTHWESTERLY LINE OF RECEPTION NO. R-93-0043076, SOUTH 45 DEGREES 29 MINUTES 38 SECONDS EAST A DISTANCE OF 66.09 FEET TO A POINT ON THE EXTENSION OF THE NORTHWESTERLY RIGHT OF WAY LINE OF SAID WAZEE STREET; THENCE SOUTH 44 DEGREES 35 MINUTES 38 SECONDS WEST ALONG THE EXTENSION OF THE NORTHWESTERLY RIGHT OF WAY LINE OF SAID WAZEE STREET AND THE SOUTHEASTERLY LINE OF SAID BLOCK C, EAST DENVER AND THE EXTENSION THEREOF A DISTANCE OF 383.31 FEET TO THE TRUE POINT OF BEGINNING, CITY AND COUNTY OF DENVER, STATE OF COLORADO.

CONTAINING 131,694 SQUARE FEET OR 3.023 ACRES MORE OR LESS.

1.1.1 Subareas Established

The following subareas are hereby established within PUD-G 19 for the purpose of applying the zoning standards contained herein, including building height and upper story setbacks. All subareas established are shown generally on Figure 1.1 below and described as follows:

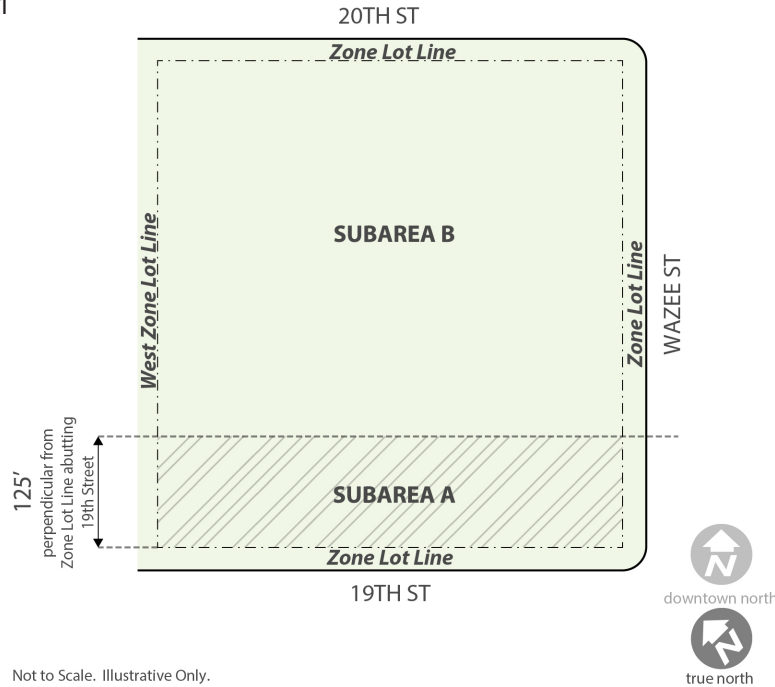
A. Subarea A

Subarea A shall be the area of PUD-G 19 within 125 feet of the Zone Lot Line abutting 19th Street as measured perpendicularly from the Zone Lot Line abutting 19th Street.

B. Subarea B

Subarea B shall be the area of PUD-G 19 not within Subarea A.

Figure 1.1



SECTION 1.2 PUD-G 19 GENERAL PURPOSE

The general purpose of PUD-G 19 is to facilitate redevelopment with mixed residential and commercial uses and amenities that are compatible with the surrounding Downtown Context and adjacent Lower Downtown Historic District and contribute to the vibrancy of the Union Station neighborhood.

SECTION 1.3 PUD-G 19 SPECIFIC INTENT

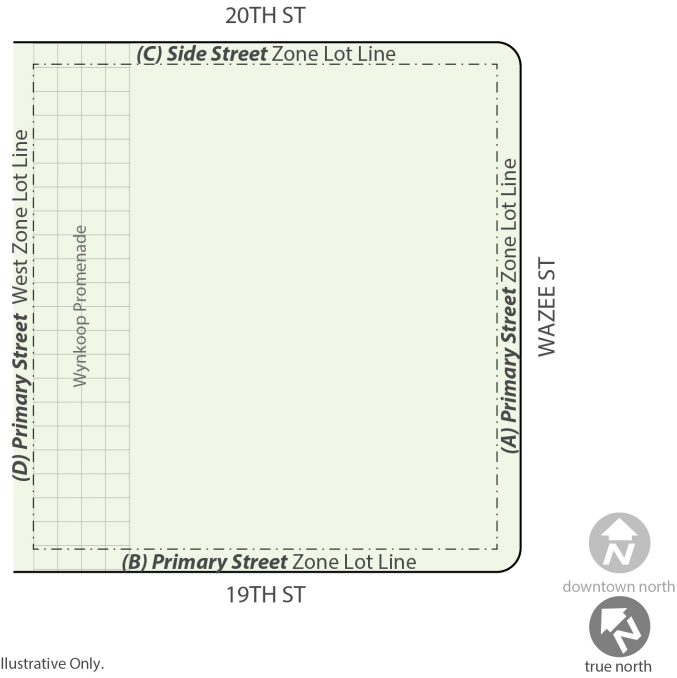
PUD-G 19 is intended to:

- 1.3.1 Allow mixed use development that contributes to the vibrancy of Downtown and the surrounding neighborhoods.
- 1.3.2 Facilitate compatible development through the use of appropriate building form and design standards and guidelines.
- 1.3.3 Encourage pedestrian-activated public spaces as envisioned in the City’s adopted plans, including but not limited to the Downtown Area Plan.
- 1.3.4 Create a key pedestrian and bicycle connection through the site, between 19th Street and 20th Street, herein referenced as “Wynkoop Promenade,” via a requirement for a permanently open and publicly accessible walkway, with building form standards that ensure it operates similarly to Downtown streets.
- 1.3.5 Ensure quality, human-scaled building design.
- 1.3.6 Provide a scale and building form transition from the Lower Downtown Historic District to 20th Street.
- 1.3.7 Address the unique conditions and location of the site, including the lack of a through street connection along the Wynkoop Street alignment and its adjacency to the Lower Downtown Historic District and Colorado Rockies baseball stadium.

SECTION 1.4 ZONE LOT ESTABLISHED

This PUD-G 19 establishes one zone lot whose boundaries are the same as the boundaries of this PUD-G 19, as shown generally on Figure 1.2 below, and legally described in Section 1.1 of this PUD-G 19. Determination of Primary and Side Street Zone Lot Lines are established in Section 6.6.3.A of this PUD-G 19.

Figure 1.2



Not to Scale. Illustrative Only.

CHAPTER 2. DOWNTOWN NEIGHBORHOOD CONTEXT DESCRIPTION

All development within this PUD-G 19 shall conform to the Denver Zoning Code, Division 8.1, Downtown Neighborhood Context Description, as amended from time to time.

CHAPTER 3. DISTRICTS

Development in this PUD-G 19 shall conform to the Denver Zoning Code, Division 8.2, Districts, as specifically applicable to the D-AS-12+ Zone District, as amended from time to time, except as modified in this PUD-G 19.

CHAPTER 4. DESIGN STANDARDS

Development in this PUD-G 19 shall comply with the Denver Zoning Code, Division 8.8, Downtown Arapahoe Square 12+ and Downtown Arapahoe Square 20+ Districts, as specifically applicable to the D-AS-12+ Zone District, as amended from time to time, with the following exceptions, additions, and modifications.

SECTION 4.1 PRIMARY BUILDING FORM STANDARDS

Section 8.8.3 of the Denver Zoning Code, Primary Building Form Standards for Downtown Arapahoe Square 12+ and Downtown Arapahoe Square 20+ Districts, shall not apply. Instead, all development in this PUD-G 19, except detached accessory structures, shall comply with the following primary building form standards.

4.1.1 Intent

A. General

1. Address the site's unique location and conditions by applying a combination of Urban Center and Downtown building form standards.
2. Facilitate pedestrian access and activity on the required Wynkoop Promenade by applying Primary Street standards to the West Zone Lot Line.

B. Building Height and Upper Story Setbacks

1. Arrange building heights at the edges of PUD-G 19 to provide a transition from the relatively lower heights allowed in the Lower Downtown Historic District.
2. Maintain the general appearance of a predominantly 8-story height near the sidewalk edge along 19th Street by requiring an Upper Story Setback at taller heights.

C. Required Private Open Space

1. Encourage the provision of publicly accessible open space that enhances pedestrian and bicycle connections, and creates activated amenity space for users and visitors.

D. Limitation on Visible Parking Above the Street Level

1. Promote structured parking designs that are compatible in design, character and quality with adjoining buildings, open spaces, and streetscapes.
2. Reduce the visual prominence of above-ground structured parking by promoting visual interest in the design of upper story building facades.

4.1.2 Applicability

All development, except detached accessory structures, in this PUD-G 19 shall comply with the primary building form standards in this Section 4.1 and Table 4.1 below.

4.1.3 District Specific Standards - PUD-G 19 Primary Building Form Standards

TABLE 4.1: PUD-G 19 PRIMARY BUILDING FORM

HEIGHT	Subarea A	Subarea B
Stories (max)	11	13
Feet (max)	150'	165'
Height Exceptions	See Section 4.5 of this PUD-G 19	
SITING	All Subareas	
REQUIRED BUILD-TO BY STREET		
West Zone Lot Line (min build-to % within min/max range)	70% 70'/85'	
Wazee Street and 19th Street (min build-to % within min/max range)	70% 0'/10'; Residential only buildings: 70% 0'/15'	
20th Street	No minimum build-to required	
Build-to Exceptions and Alternatives	See Sections 4.4 and 4.5 of this PUD-G 19	
SETBACKS		
Primary Street, Side Street (min)	0'	
PARKING		
Surface Parking between building and Primary Street/Side Street	Not allowed	
Surface parking screening required	See Section 10.5.4.4 of the Denver Zoning Code, Perimeter Surface Parking Lot Landscaping Standards	
Vehicle Access	Shall be determined as part of Site Development Plan Review	
DESIGN ELEMENTS	Subarea A	Subarea B
REQUIRED PRIMARY STREET UPPER STORY SETBACK BY STREET		
Wazee Street and 19th Street - Upper Story Setback above 110' and 8 stories	15'	No Upper Story Setback required
20th Street and West Zone Lot Line	No Upper Story Setback required	
REQUIRED PRIVATE OPEN SPACE		
Private Open Space (min)	20%	
STREET LEVEL ACTIVATION		
Street Level Transparency, Primary Street (min)	60% Residential only buildings: 40%	
Street Level Transparency, Side Street (min)	25%	
Street Level Transparency Alternatives	See Section 4.4 of this PUD-G 19	
Pedestrian Access, Primary Street	Entrance	
LIMITATION ON VISIBLE PARKING ABOVE STREET LEVEL		
Limitation on Visible Parking above Street Level, Primary Street (min % of Primary Street-facing zone lot width)	70%	
USES	All Subareas	
<p>(1) All permitted Primary Uses shall be allowed within this building form. See Chapter 5, Uses and Parking, of this PUD-G 19; and (2) 100% of the portion of the Street Level building frontage that meets the minimum Primary Street build-to requirement shall be occupied by Street Level active uses as described in Section 8.8.5.1 of the Denver Zoning Code, as applicable to the D-AS-12+ district.</p>		

SECTION 4.2 DETACHED ACCESSORY BUILDING FORM STANDARDS

Development in this PUD-G 19 shall comply with the Detached Accessory Building Form Standards in Section 8.8.4 of the Denver Zoning Code as applicable to the D-AS-12+ Zone District, as amended from time to time.

SECTION 4.3 SUPPLEMENTAL DESIGN STANDARDS

Development in this PUD-G 19 shall comply with the Supplemental Design Standards for Downtown Arapahoe Square 12+ and Downtown Arapahoe Square 20+ Districts in Section 8.8.5 of the Denver Zoning Code as applicable to the D-AS-12+ Zone District, as amended from time to time, with the following modification:

4.3.1 Limitation on Visible Parking Above Street Level

Section 8.8.5.2, of the Denver Zoning Code, Limitation on Visible Parking Above Street Level in the D-AS-12+ and D-AS-20+ Zone Districts, as amended from time to time, shall apply to the PUD-G 19 Primary Building Form.

SECTION 4.4 DESIGN STANDARD ALTERNATIVES

Development in this PUD-G 19 shall comply with the Design Standard Alternatives in Section 8.8.6 of the Denver Zoning Code as applicable to the D-AS-12+ Zone District, as amended from time to time, with the following exceptions, additions and modifications:

4.4.1 Required Build-to Alternatives

A. Intent

Help define the public realm and enhance the visual quality of the built environment where it is not possible to define the street and public sidewalk edge with building facades.

B. Requirements

Section 8.8.6.1 of the Denver Zoning Code, Required Build-To Alternatives in D-AS-12+ and D-AS-20+ Districts, shall not apply. Instead, the following alternatives may be used as an alternative to a required build-to standard and may count toward the required build-to no more than as described in Table 4.2 below. All alternatives shall meet the Build-to Alternative Requirements in Section 13.1.5.7.E of the Denver Zoning Code, as amended from time to time:

ZONE DISTRICT	PRIVATE OPEN SPACE (MAX % OF BUILD-TO)	GARDEN WALL (MAX % OF BUILD-TO)	GARDEN WALL WITH COVERED SEATING FOR PEDESTRIANS (MAX % OF BUILD-TO)	ARCADE (MAX % OF BUILD-TO)
PUD-G 19	25%	25%*	30%*	100%

*If used in combination with each other, the garden wall and garden wall with covered seating for pedestrians alternatives may count toward no more than 30% of required build-to.

4.4.2 Transparency Alternatives

A. Intent

1. Provide visual interest on building facades to activate the public realm and enhance the visual quality of the built environment along Street Level facade areas where windows do not provide sufficient transparency.
2. Provide visual interest and activation along 20th Street while accommodating a reasonable amount of service uses that are unable to locate along an alley.

B. Requirements

Section 8.8.6.3 of the Denver Zoning Code, Transparency Alternatives, shall not apply. Instead, the following alternatives may be used singularly or in combination as alternatives to a required transparency standard and may count toward required transparency no more than

as described in Table 4.3 below. All alternatives shall meet the Transparency Alternative Requirements in Section 13.1.6.3.A.5 of the Denver Zoning Code, as amended from time to time:

TABLE 4.3: TRANSPARENCY ALTERNATIVES					
ZONE DISTRICT	ZONE LOT LINE DESIGNATION	DISPLAY CASES (MAX)	PERMANENT ART (MAX)	PERMANENT OUTDOOR EATING/SERVING AREAS	COMBINATION OF ALTERNATIVES (MAX)
PUD-G 19	Primary Street	20%	20%	20%	40%
	Side Street	40%	40%	50%	50%

SECTION 4.5 DESIGN STANDARD EXCEPTIONS

Development in this PUD-G 19 shall comply with the Design Standard Exceptions in Article 8 of the Denver Zoning Code as applicable to the D-AS-12+ Zone District, as amended from time to time, with the following exceptions, additions and modifications:

4.5.1 Height Exceptions

A. Intent

To facilitate active use of the rooftop created at a required Upper Story Setback.

B. Parapet Wall or Safety Railing

A parapet wall and/or safety railing shall be allowed to encroach into the Upper Story Setback by any distance.

CHAPTER 5. USES AND PARKING

SECTION 5.1 USES

5.1.1 Primary, accessory and temporary uses allowed in this PUD-G 19 shall be those same uses allowed in the D-AS-12+ Zone District, as stated in the Denver Zoning Code, Division 8.10, Uses and Required Minimum Parking, as amended from time to time.

SECTION 5.2 REQUIRED MINIMUM PARKING

5.2.1 There shall be no minimum off-street vehicle parking requirement for any use in this PUD-G 19.

5.2.2 All uses established in this PUD-G 19 shall comply with the bicycle parking requirements in the District Specific Standards in the Denver Zoning Code, Division 8.10, as amended from time to time, as applicable to specific uses allowed in the D-AS-12+ Zone District.

SECTION 5.3 MAXIMUM VEHICLE PARKING

5.3.1 Intent

- A. Promote active, transit-supportive development and uses proximate to public transit, thereby leveraging regional investment in transit infrastructure and promoting livability and sustainability in the Downtown context and the Union Station neighborhood.
- B. Reduce the number of vehicle trips within and around the zone district and encourage alternative travel modes such as walking, biking, and transit.
- C. Promote efficient development by limiting the amount of land and building area dedicated to vehicle parking.
- D. Establish an overall vehicle parking maximum for PUD-G 19 based on maximum parking ratios for anticipated use types, including .8 spaces per residential unit, .25 spaces per guest room for lodging accommodations, 1.25 spaces per 1,000 square feet for office uses, and 1 space per 1,000 square feet for other commercial uses.

5.3.2 Vehicle Parking Standards

No more than 475 vehicle parking spaces shall be allowed in this PUD-G 19.

CHAPTER 6. ADDITIONAL STANDARDS

SECTION 6.1 ARTICLE 1 OF THE DENVER ZONING CODE

6.1.1 Applicability

Development in this PUD-G 19 shall conform to Denver Zoning Code Article 1, General Provisions, as amended from time to time.

SECTION 6.2 ARTICLE 8 OF THE DENVER ZONING CODE

6.2.1 Applicability

Development in this PUD-G 19 shall comply with the Denver Zoning Code Article 8, Downtown Neighborhood Context, as specifically applicable to the D-AS-12+ Zone District, as amended from time to time, except as modified by this PUD-G 19.

SECTION 6.3 ARTICLE 10 OF THE DENVER ZONING CODE

6.3.1 Applicability

Development in this PUD-G 19 shall comply with the Denver Zoning Code, Article 10, General Design Standards, as specifically applicable to the D-AS-12+ Zone District, as amended from time to time, except as modified in Chapter 5 above and with the following additions:

6.3.2 General Site Design and Facility Standards

A. Wynkoop Promenade

1. Intent

Design and operate the Wynkoop Promenade similar to a public street, facilitating public pedestrian and bicycle access to and through the zone district, and supporting special events.

2. Requirements

- a. A pedestrian walkway, herein referred to as the “Wynkoop Promenade,” connecting the 19th Street right-of-way to the 20th Street right-of-way, shall be provided within the area located 80 feet from the West Zone Lot Line, as measured perpendicularly from the West Zone Lot Line.
- b. The pedestrian walkway shall provide not less than 15 feet clear walking area.
- c. The pedestrian walkway shall remain publicly accessible and usable at all times, except that it may be closed to accommodate special events in accordance with permitting requirements of the Denver Zoning Code, Division 8.10, Uses and Required Minimum Parking, as amended from time to time, as specifically applicable to the D-AS-12+ Zone District.

SECTION 6.4 ARTICLE 11 OF THE DENVER ZONING CODE

6.4.1 Applicability

Establishment of uses in this PUD-G 19 shall comply with the Denver Zoning Code, Article 11, Use Limitations and Definitions, as amended from time to time, as specifically applicable to the D-AS-12+ Zone District.

SECTION 6.5 ARTICLE 12 OF THE DENVER ZONING CODE

6.5.1 Applicability

All development in this PUD-G 19 shall comply with the Denver Zoning Code, Article 12, Procedures and Enforcement, as amended from time to time, with the following exceptions, additions, and modifications:

6.5.2 Zone Lot Amendments

A. Intent

Facilitate a comprehensive approach to Private Open Space and pedestrian access goals in PUD-G 19.

B. Requirements

The Zoning Administrator shall deny any zone lot amendment application that would result in more than one zone lot within the PUD-G 19 zone district.

SECTION 6.6 ARTICLE 13 OF THE DENVER ZONING CODE

6.6.1 Applicability

Development in this PUD-G 19 shall comply with the Denver Zoning Code, Article 13, Rules of Measurement and Definitions, as amended from time to time, with the following exceptions, additions, and modifications:

6.6.2 Rules of Measurement for Height

Development in this PUD-G 19 shall comply with Denver Zoning Code, Section 13.1.3, Height for All Other Zone Districts, as amended from time to time, with the following modification:

A. Base Plane Established

1. Overall height for all buildings and structures on the zone lot shall be measured from a base plane.
2. Base plane rules of measurement in Section 13.1.3.2 of the Denver Zoning Code shall not apply. Instead, a single base plane shall be established at an elevation of 5189'-3" above sea level (NAVD 88) for the purpose of measuring maximum height.

6.6.3 Rules of Measurement for Siting Form Standards

A. Determination of Primary and Side Street Zone Lot Lines

1. Intent

- a. Facilitate pedestrian access and activity on the Wynkoop Promenade by applying Primary Street standards to the West Zone Lot Line.
- b. Determine Zone Lot Lines to address the site's unique attributes, including prominent frontages on 19th Street, Wazee Street, and the Wynkoop Promenade; lack of an alley; and substantial grade changes along 20th Street.

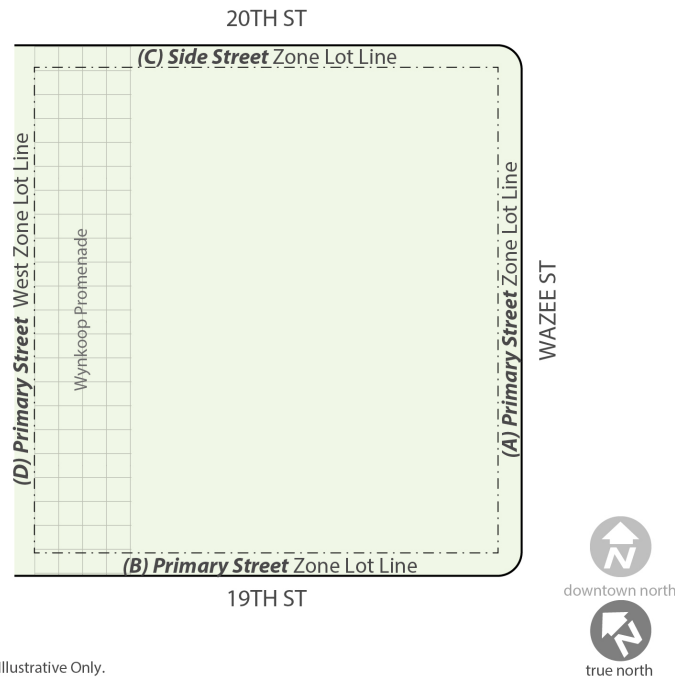
2. Requirements

Determination of Zone Lot Lines provide a reference for measurement of standards related to building form and placement (e.g. build-to, transparency, street level activation, Private Open Space), as referenced in Chapters 4 and 6 of this PUD-G 19. Section 13.1.5.5 of the Denver Zoning Code, Determination of Primary Street, Side Interior, and Rear Zone Lot Lines for all C-CCN, D-AS-12+ and D-AS-20+ Zone Districts, shall not apply. Instead, the following Zone Lot Line determinations shall apply to development within PUD-G 19, and administrative adjustments or variances to such determinations are prohibited:

- a. The Zone Lot Line abutting Wazee Street is designated as a Primary Street Zone Lot Line ((A) on the reference graphic, Figure 6.1).

- b. The Zone Lot Line abutting 19th Street is designated as a Primary Street Zone Lot Line ((B) on the reference graphic, Figure 6.1).
- c. The Zone Lot Line abutting 20th Street is designated as a Side Street Zone Lot Line ((C) on the reference graphic, Figure 6.1).
- d. The Zone Lot Line abutting the west boundary of this PUD-G 19 district is designated as a Primary Street Zone Lot Line ((D) on the reference graphic, Figure 6.1), and herein referenced as the “West Zone Lot Line.”

Figure 6.1



Not to Scale. Illustrative Only.

B. Build-To Alternative Requirements

1. Intent

Encourage visually interesting, activated, and pedestrian-scaled open space, while accommodating below-grade parking.

2. Private Open Space Build-to Alternative

Private Open Space used as a build-to alternative in this PUD-G 19 shall comply with the standards for Private Open Space Build-To Alternatives in Section 13.1.5.7.E.2 of the Denver Zoning Code, as amended from time to time, with the following exceptions and additions:

- a. The rule of measurement that a Private Open Space used as a build-to alternative shall be open to the sky shall apply, except that canopies and awnings may encroach into and over the Private Open Space.
- b. The rule of measurement that a Private Open Space used as a build-to alternative shall be fully visible from a Primary Street shall not apply. Instead, Private Open Space used as a build-to alternative shall be fully visible from a Primary Street Zone Lot Line as designated in Section 6.6.3 of this PUD-G 19.
- c. Private Open Space may be located above a below-grade parking structure.

C. Required Private Open Space Standards

1. Intent

- a. Encourage visually interesting, activated, and pedestrian-scaled Private Open Space, while accommodating below-grade parking.
- b. Ensure that Private Open Space is publicly accessible, while still allowing the use of Private Open Space for special events and programming.
- c. Enhance pedestrian and bicycle connections through the PUD-G 19 zone district with the siting of Private Open Space.

2. Rules of Measurement

Development in this PUD-G 19 shall meet the Private Open Space standards of Section 13.1.6.1.B of the Denver Zoning Code, as amended from time to time, with the following additions and modifications:

- a. The rule of measurement that a Private Open Space shall be fully visible from a Primary Street or Side Street shall not apply. Instead, Private Open Space shall be fully visible from either a Primary or Side Street Zone Lot Line as designated in Section 6.6.3 of this PUD-G 19.
- b. Canopies and awnings are allowed within the total area open to the sky.
- c. Private Open Space may be located above a below-grade parking structure.
- d. The rules of measurement requiring at least one Minimum Contiguous Area shall not apply.
- e. Private Open Space shall include the Wynkoop Promenade pedestrian walkway, subject to the requirements of Section 6.3.2 of this PUD-G 19.
- f. Private Open Space may include Private Open Space used as a build-to alternative.

CHAPTER 7. STANDARDS AND GUIDELINES

7.6.1 Intent

The Intent of this Chapter 7, Standards and Guidelines, is to:

- A. Facilitate building and site design that is compatible with the Downtown Context and adjacent Lower Downtown Historic District and achieves fluid transitions.
- B. Ensure that design promotes a vibrant sense of place and is scaled to primarily encourage an active pedestrian environment.
- C. Ensure that open spaces are activated and designed to encourage and support public use.
- D. Encourage innovative and unique design solutions that will help define a unique future character.
- E. On facades immediately adjacent to the Lower Downtown Historic District, promote building designs that are compatible with the traditional building hierarchies of the district, including but not limited to the rectilinear and blocky nature of buildings, the block-alley-block pattern on numbered streets, varied articulation on named streets, base-middle-top composition, and the extensive use of masonry.

7.6.2 Applicability

- A. Development in this PUD-G 19 shall comply with this Chapter 7, Standards and Guidelines.
- B. The Zoning Administrator may allow compliance through alternative means other than strict compliance with the specific standards and guidelines in this Chapter 7, but only when the Zoning Administrator finds that the alternative compliance meets or exceeds the applicable guiding principles and intent statements.

Chapter 7. PUD-G 19 Standards and Guidelines



ORGANIZATION & FORMAT

The Standards and Guidelines for PUD-G 19 are organized into two sections that address different levels of building and site design.

Each section begins with a set of guiding principles. Each topic within the section is then addressed at three levels:

1. **Intent Statements** establish the objectives to be achieved for each topic and may also be used to determine the appropriateness of alternative or innovative approaches that do not meet specific design standards.
2. **Design Standards** set prescriptive criteria for achieving the intent statements. They use the term “shall” to indicate that compliance is required and are numbered by section for reference.

3. **Design Guidelines** provide additional suggestions to achieve the intent statements. They use the term “should” or “consider” and are numbered by section for reference.

See [Figure 7.1, Sample Standards & Guidelines Format](#) for more detail regarding the format and use of guiding principles, intent statements, design standards and design guidelines.

APPLICATION OF THE STANDARDS & GUIDELINES

Not all standards and guidelines will apply to every project. Standards and guidelines that refer to design topics or elements that are not part of a development or redevelopment project are not applicable.

SAMPLE STANDARDS & GUIDELINES FORMAT

To increase clarity and ease-of-use, the individual standards and guidelines pages in Sections 1-2 use a standard format. The chart below uses a sample page to indicate each key element of the standard format.

A → **Street Level Design**

B → **Building Entries**

C → 

25. Pedestrian entrances shall front onto a public street or street-facing Open Space.

E → **DESIGN STANDARDS**

2.23 Primary pedestrian entrances shall front onto a public street or Open Space.

2.24 The design of primary entries shall respond to the Street Level building use.

G →

- a. Locate commercial entrances at the level of the adjacent sidewalk whenever possible.
- b. Locate residential entrances no more than approximately 3 feet above the level of the adjacent sidewalk.

F → **DESIGN GUIDELINES**

2.30 Primary pedestrian entrances should be integrated into a signature building element whenever possible.

2.31 Where transit stops are adjacent to a building, a pedestrian entrance should be located adjacent to the stop.

2.32 For buildings with multiple tenants, façades should be divided into narrow widths or bays and provide multiple secondary access points to animate the street.

D → **INTENT STATEMENTS**

- To activate the Street Level and integrate pedestrian circulation into building design.
- To ensure that pedestrian entries are clearly visible.

H → **PUD-G 19 PEDESTRIAN ACCESS REQUIREMENTS**

Chapter 4 of this PUD-G 19 includes pedestrian access (entrance) requirements to ensure a clear, obvious, publicly accessible connection between the primary street and uses within the building.

KEY TO THE SAMPLE STANDARDS AND GUIDELINES PAGE ABOVE



- A** **The Design Topic** is indicated with a heading at the top of each page.
- B** **A Design Subtopic** is sometimes included in black text at the right side of the header.
- C** **Photographs & Diagrams** are located below the page heading. They are numbered for easy reference.
-  **A Checkmark** indicates an appropriate approach
-  **An X mark** indicates an inappropriate approach
- D** **Intent Statements** establish the objectives to be achieved for each topic and may also be used to determine the appropriateness of alternative or innovative approaches that do not meet specific design standards.
- E** **Design Standards** set prescriptive criteria for achieving the intent statements. They use the term “shall” to indicate that compliance is expected and are numbered by section for reference.
- F** **Design Guidelines** provide additional suggestions to achieve the intent statements. They use the term “should” or “consider” and are numbered by section for reference.
- G** **Additional Information** is provided as a bulleted list beneath some standards and guidelines to indicate specific approaches and strategies or an additional standard or guideline.
- H** **Sidebars** provide background information on the design topic or relationship to other Chapters of PUD-G 19 or the Denver Zoning Code.
- I** **Underlined Text** indicates terms defined in the Appendix, or provides cross references to related document topics and web sites.

Figure 7.1 Sample Standards & Guidelines Format

Chapter 7, Section 1: Site Design Standards & Guidelines



Site design addresses the arrangement of buildings and the spaces on a site, as well as the visual and functional character of those spaces and how they shape the Public Realm.

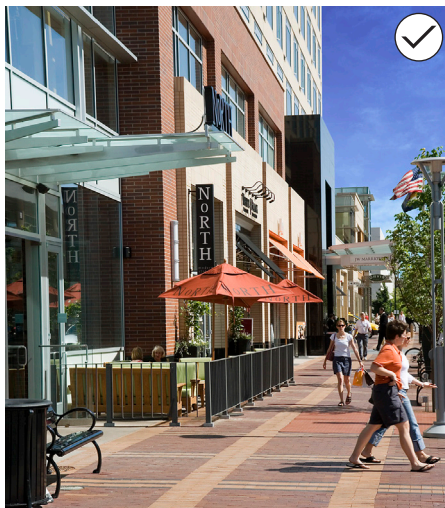
This section provides design standards and guidelines for site design topics, including street frontage, Open Space, surface parking and service areas. The design standards and guidelines apply to site improvement.

GUIDING PRINCIPLES FOR SITE DESIGN

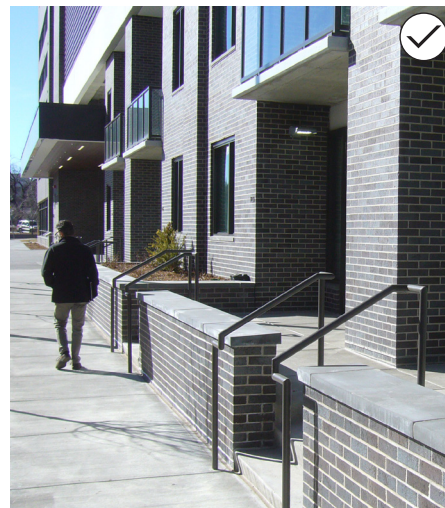
The following core site design principles provide the basis for the standards and guidelines:

- **Sense of Place.** Site designs that frame the street and sidewalks with buildings, Enhanced Setback areas and Open Space help create active edges and provide a sense of comfort and safety that promotes a strong sense of place.
- **Human Scale.** Site designs that provide respite from the busy urban environment and encourage pedestrian activity help promote a sense of Human Scale.
- **Creativity.** Site designs that incorporate creative features help to define a unique future character.
- **Context.** Site designs that are compatible with their context and help ensure harmonious relationships with adjacent neighborhoods.
- **Sustainability.** Site designs that encourage pedestrian interaction and incorporate low impact development principles promote social, economic and environmental sustainability.

Street Frontage



1. Commercial frontages should activate adjacent sidewalks.



2. Residential frontages should provide a transition between adjacent sidewalks and private residences.

INTENT STATEMENTS

- To locate and orient buildings to create a well-defined street frontage that promotes a vibrant pedestrian experience
- To provide a variety of experiences along the street frontage
- To link the street frontage to activities in adjacent buildings

DESIGN STANDARD

1.01 Buildings shall be located to clearly define a pedestrian-oriented street frontage.

One or more of the following frontage conditions are appropriate:

- a. Buildings located at or near the Primary Street Zone Lot Line to frame the sidewalk.
- b. An Enhanced Setback to extend the public use area
- c. An Open Space area such as a plaza or courtyard. See Open Space Standards and Guidelines 1.04-1.22.

DESIGN GUIDELINES (Continued)

1.03 Residential frontages should provide a transition between adjacent sidewalks and private residences.

Use one or more transitional features, such as:

- a. Building located at or near the Primary Street Zone Lot Line with primary entrance/lobby areas at the sidewalk edge
- b. A landscaped Enhanced Setback area
- c. Open Spaces such as courtyards
- d. Stoops or small private yard areas

DESIGN GUIDELINES

1.02 Commercial frontages should activate adjacent sidewalks.

Use one or more of the following features:

- a. Buildings located at or near the Primary Street Zone Lot Line with Street Level Active Uses at the sidewalk edge
- b. Enhanced Setback areas with pedestrian seating, outdoor dining, or an extended sidewalk
- c. Open Spaces such as plazas that are directly connected to building entries and Highly Active Uses

PEDESTRIAN-ORIENTED STREET FRONTAGE

The strategies described and illustrated below are appropriate to clearly define a pedestrian-oriented street frontage.

A. BUILDINGS LOCATED AT OR NEAR THE PRIMARY STREET ZONE LOT LINE



Buildings located at or near the Primary Street Zone Lot Line directly activate the street and sidewalk with building entries and activities. See *Street Level Design* for related building standards and guidelines.

B. ENHANCED SETBACKS



An *Enhanced Setback* is the space created when buildings are set back from the Primary Street property line, but generally still positioned within the Primary Street build-to range provided in PUD-G 19. Such setbacks should be enhanced to provide attractive and usable areas that activate the street frontage. They can range in size from modest extensions of the sidewalk that provide additional pedestrian area to larger landscape or patio seating areas.

C. OPEN SPACE

Open Spaces extend beyond the Primary Street build-to range allowed by PUD-G 19 to provide expanded pedestrian use areas and activated public spaces. They are typically located adjacent to the street frontage or along *Wynkoop Promenade*. *Open Space* may include courtyards & plazas, pocket parks and pedestrian paths leading into the site.

Note that the *Open Space* used to meet the *Private Open Space* required in PUD-G 19 must meet the standards and guidelines.



Figure 7.2 Pedestrian-oriented Street Frontage and Open Space

Street Frontage

Enhanced Setbacks & Open Space



3. Enhanced Setback and Open Space areas are intended to ensure that areas where buildings are not built directly along the sidewalk edge contribute to activating the Public Realm.

INTENT STATEMENTS

- To ensure that Open Space and Enhanced Setbacks activate the Public Realm
- To encourage additional sidewalk space for pedestrian movement and door openings
- To encourage a variety of Open Spaces, such as courtyards, plazas, and pocket parks
- To provide comfortable and vibrant space for publicly accessible outdoor amenities, such as café seating, event space, play features, and public art
- To promote an active pedestrian connection between the Public Realm and private development
- To ensure that Enhanced Setbacks and Open Spaces provide a sense of comfort and safety for pedestrians
- To facilitate public pedestrian and bicycle movement through and around the zone district

DESIGN STANDARDS

- 1.04 Open Spaces, such as courtyards and plazas, shall be fronted with Highly Active Uses on at least one side.
- Highly Active Uses include, but are not limited to:
- a. Retail storefronts
 - b. Restaurants and cafes
 - c. Building lobbies and amenity areas
 - d. Indoor art or recreation facilities
 - e. Arts and cultural facilities
- 1.05 Wynkoop Promenade and the 20th Street frontage shall be designed to accommodate a 15 foot clear path for public pedestrian and bicycle access through and around the zone district.
- 1.06 Where Open Spaces, such as courtyards and plazas are provided, they shall be located adjacent to pedestrian building entries or along routes that lead to building entries.
- 1.07 Open Spaces, such as courtyards and plazas, shall be located and oriented to provide a visual connection to the street.
- 1.08 Enhanced Setback and Open Space areas shall include integrated pedestrian-scale lighting to encourage evening use and to enhance security.

1.09 Facades adjacent to Enhanced Setback and Open Space areas shall incorporate features that promote pedestrian safety and activation.

Such features include, but are not limited to:

- a. Clear sight lines
- b. Adequate, pedestrian-scaled, lighting
- c. Transparent windows linked to building uses
- d. A minimum of 12 feet for residential and 14 feet for commercial ground floor floor-to-floor heights that promote visual interest and pedestrian safety

1.10 Enhanced Setbacks and Open Space areas shall incorporate features to enhance year-round usability.

Features may include, but are not limited to:

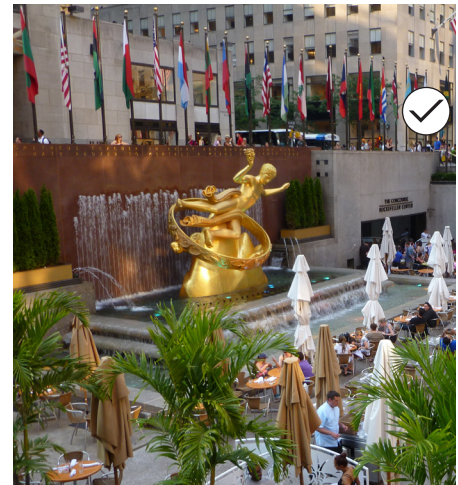
- a. Trees, canopies, awnings, or other features that provide shade where an Open Space is exposed to the summer sun
- b. Seating areas designed and oriented to provide winter warmth where an Open Space may be shaded in the winter

Street Frontage

Enhanced Setbacks & Open Space (continued)



4. *Enhanced Setback and Open Space areas should be designed to complement adjacent building uses.*



5. *Where possible, public art should be integrated into an Enhanced Setback or Open Space area.*

DESIGN GUIDELINES

1.11 Enhanced Setback and Open Space areas should be designed to complement adjacent building uses.

Complementary designs include:

- a. Enhanced Setback areas that provide seating for customers of adjacent commercial storefronts
- b. Outdoor dining areas adjacent to a cafe or restaurant
- c. Landscaped courtyards with integrated seating to complement adjacent residential or office uses
- d. Stoops or small yard areas to complement adjacent residential units

1.12 Enhanced Setbacks and Open Space areas should provide both formal and informal seating areas.

Formal seating may include, but is not limited to:

- a. Integrated benches
- b. Movable chairs or benches

Informal seating may include, but is not limited to:

- c. Planter ledges that provide seating
- d. Bollards or planters

1.13 Enhanced Setback areas should serve as an extension of the public sidewalk.

- a. Use materials that are similar to the adjacent sidewalk.

b. Consider modest setbacks that add pedestrian use area.

1.14 Pedestrian areas that are part of Enhanced Setbacks and Open Space should be located at grade with the sidewalk around the zone district.

Note that areas intended for private residential use may be located above or below the sidewalk level. See design standard 2.23 for more information on raised residential entries.

1.15 Pedestrian areas that are part of Enhanced Setbacks and Open Space should be paved with high-quality, durable materials.

1.16 Trees and plantings in an Enhanced Setback or Open Space area should be hardy and drought tolerant.

1.17 Enhanced Setback and Open Space areas should incorporate sustainable stormwater management systems, where possible.

1.18 Open Space should be designed to accommodate events such as outdoor markets or concerts, where possible.

1.19 Public art should be integrated into an Enhanced Setback or Open Space area, where possible.

1.20 Additional private amenity areas, such as decks, should be provided for residents and employees.

1.21 Open Space should be designed to accommodate a variety of activities for all ages with design elements such as interactive play features and seating areas.

1.22 The design of Wynkoop Promenade and the 20th Street frontage should include pedestrian and bicycle wayfinding features, such as special paving or signage.

Vehicle Access



6. Vehicle access points shall be designed to minimize impacts on pedestrians.



7. Vehicle access points shall not be located immediately adjacent to Open Space area.

INTENT STATEMENTS

- To minimize conflicts between vehicles, pedestrians and cyclists
- To reduce the visual impacts of vehicle access on the Public Realm
- To protect sidewalks, Enhanced Setback areas, Open Spaces and other pedestrian-oriented areas from vehicular impacts

PUBLIC WORKS REVIEW

Vehicle access is subject to review and approval by the City of Denver's Department of Public Works. In some cases, Public Works review may result in required changes that deviate from the standards and guidelines.

DESIGN STANDARDS

- 1.23 The number of street access points shall depend on lot frontage.
- a. Zone Lot Lines with less than approx. 300 feet of street frontage: 0-1 access points
 - b. Zone Lot Lines with more than approx. 300 feet of street frontage: 1-2 access points
- 1.24 Vehicle access points shall be designed to minimize impacts on pedestrians.
- Appropriate strategies include:
- a. Limiting the width of vehicle access points
 - b. Using special paving materials to differentiate pedestrian and vehicle use areas
 - c. Consolidating vehicle access points with service areas
 - d. Recessing vehicle access areas
- 1.25 Vehicle access points shall not be located immediately adjacent to an Open Space.

DESIGN GUIDELINES

- 1.26 Vehicle access points should be located and designed to maintain an active street edge.
- Appropriate strategies include:
- a. Limiting the width of vehicle access points
 - b. Locating active building floors directly above vehicle access points
- 1.27 Vehicle access doors within 15 feet of the Public Right-of-Way should incorporate high-quality materials and finishes that are consistent with the building.

VEHICLE ACCESS OPTIONS

Site designs that minimize conflicts between vehicles, pedestrians and cyclists will help ensure a sense of place. As illustrated below, vehicle access locations and designs that protect pedestrians while reducing visual impacts on the Public Realm are desired. See Public Works Review on page 23 for more information regarding approval of vehicular access points.

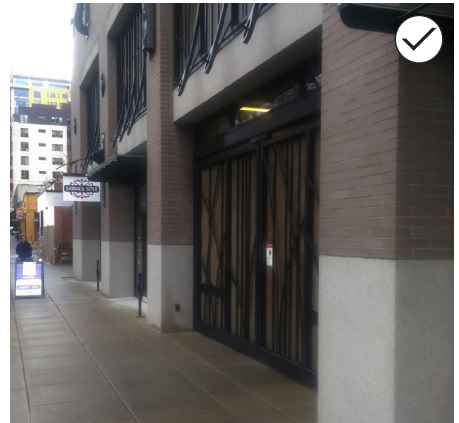
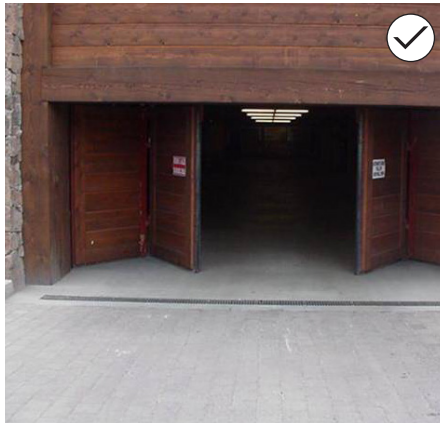
LIMITED ACCESS WIDTH



RECESSED VEHICLE ACCESS DOORS



CONSOLIDATION WITH UTILITIES



Limiting the width of vehicle access points can help minimize impacts on pedestrians.

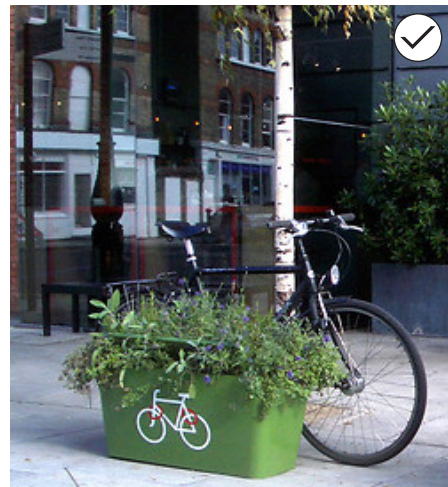
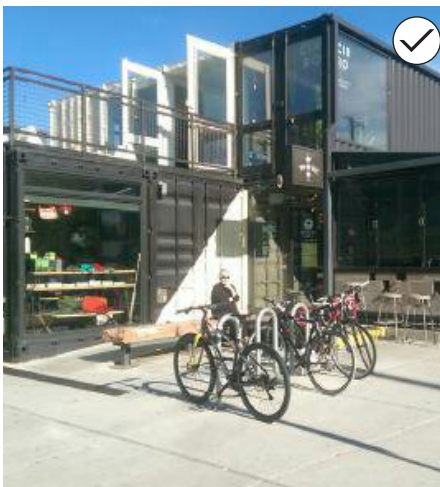
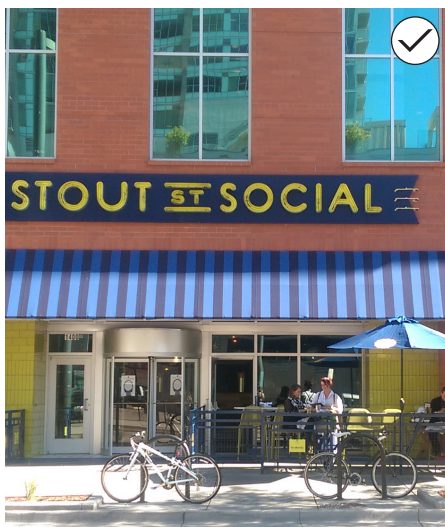
Recessing vehicle access doors from the street can help mitigate visual impacts.

Consolidating vehicle and utility access areas can help minimize pedestrian impacts.

Figure 1: Vehicle Access Options

Parking

Bicycle Parking



8. Bicycle Parking should be located adjacent to Highly Active Uses to increase security and natural surveillance.

9. Bicycle parking that is provided in addition to minimum PUD-G 19 requirements should incorporate creative designs.

INTENT STATEMENTS

- To promote sufficient bicycle parking, especially adjacent to existing and planned bicycle facilities
- To ensure that bicycle parking is located in a highly visible and accessible area near pedestrian circulation
- To ensure that bicycle parking is safe and secure
- To promote a multi-modal network

DESIGN STANDARDS

- 1.28 Bicycle parking shall be located in active areas that are visible from the Public Right-of-Way or an Open Space.
- Locate bicycle parking:
- Within 50 feet of a pedestrian entry; and/or
 - In an Amenity Zone
 - In an Enhanced Setback or Open Space (in a way that compliments the design and functionality of the space)
- 1.29 Bicycle racks shall not impede pedestrian traffic.
- 1.30 Bicycle racks shall be located a minimum of 4 feet from street trees.

DESIGN GUIDELINES

- 1.31 Additional bicycle parking, beyond PUD-G 19 minimums, should be provided whenever possible.
- Additional bicycle parking is especially important adjacent to:
- Dedicated bicycle facilities
 - High traffic pedestrian areas
- 1.32 Bicycle Parking should be located adjacent to Highly Active Uses to increase security and natural surveillance.
- 1.33 Bicycle parking provided in addition to minimum PUD-G 19 requirements should incorporate creative designs.
- Consider:
- Creative place making
 - Integration of public art
 - Use of Bicycle Corrals

PUD-G 19 BICYCLE PARKING REQUIREMENTS

Chapter 5 of this PUD-G 19 and the Denver Zoning Code (DZC) provide specific requirements for bicycle parking. The standards and guidelines in this section are intended to supplement Chapter 5 and DZC requirements with additional guidance regarding the placement and character of bicycle parking. They are also intended to encourage the provision of additional bicycle parking beyond minimum requirements.

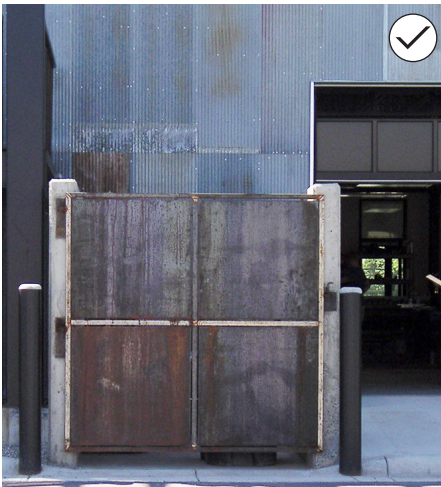
PUBLIC WORKS BICYCLE FACILITY REQUIREMENTS

The City of Denver's Department of Public Works provides standards for the design of required bicycle facilities. Public Works also requires a permit for placement of bicycle parking in the Public Right-of-Way.

See Public Work's Bicycle Parking Standards for specific dimensions and spacing requirements.

Note that the design of bicycle parking located in the public-right-of-way will be subject to approval by the City of Denver's Department of Public Works.

Service Areas & Utilities



10. Dumpsters shall be recessed and screened with high-quality materials and/or landscaping that are consistent with the building.

INTENT STATEMENTS

- To minimize the visibility and impact of service areas to the Public Realm
- To reduce conflicts between servicing activities, pedestrians and cyclists
- To protect Enhanced Setback areas, Open Spaces and other highly pedestrian-oriented areas from noise and odor impacts associated with service areas
- To encourage utility and service areas to be consolidated with other vehicle access points

PUBLIC UTILITY REQUIREMENTS

The City of Denver's local utility provider, Xcel Energy, must approve utility locations. The utility provider also reserves the right to install utilities in permanent on-site locations. In some cases, the utility provider review may result in required changes that deviate from the standards and guidelines.

DESIGN STANDARDS

- 1.34 Service, utility and loading areas shall be located within the building mass and away from pedestrian areas such as sidewalks or Open Space.
- 1.35 Service, utility and loading areas shall not be located adjacent to Open Space areas or pedestrian entries.
- 1.36 Adequate service area lighting shall be provided to promote safety.
- 1.37 Dumpsters shall be located and designed to promote safety and minimize visual impacts.
 - a. Recess dumpsters from the Public Right-of-Way.
 - b. Screen dumpsters with high-quality materials and/or landscaping that is consistent with building design.
 - c. Secure dumpsters to prevent access by non-service personnel.
- 1.38 Rooftop mechanical, electrical and telecommunications systems shall be screened to minimize visibility from the street.

DESIGN GUIDELINES

- 1.39 Ventilation shafts, grates, and other above-ground mechanical or site servicing equipment, should be located away from the Public Realm. Where it is not feasible to locate away from the Public Realm, they should be designed to minimize impacts on the Public Realm.
- 1.40 Service areas should be combined with vehicle access points when possible to minimize pedestrian and Public Realm impacts.

Also see Standard 1.27.

Chapter 7, Section 2: Building Design Standards & Guidelines



Building design addresses the vertical component of development and redevelopment, which includes the visual and functional character of individual buildings.

This section provides standards and guidelines for key building design topics, including building massing, pedestrian character and materials.

GUIDING PRINCIPLES FOR BUILDING DESIGN

The following core building design principles provide the basis for the standards and guidelines:

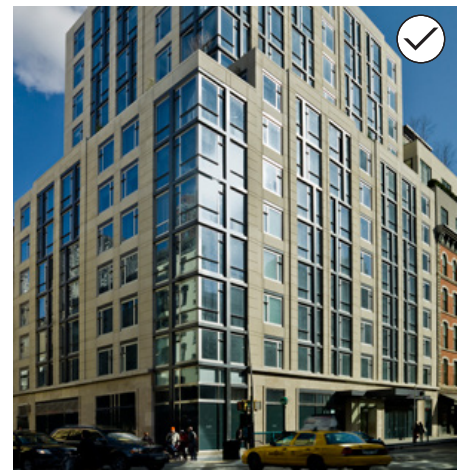
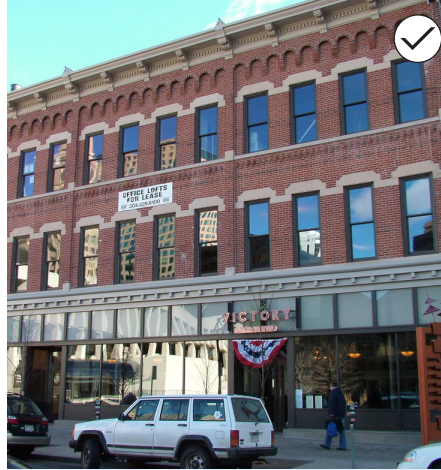
- **Sense of Place.** Buildings that are designed to frame and relate to the Public Realm help promote a vibrant sense of place.
- **Human Scale.** Buildings that are scaled and designed to encourage pedestrian activity promote a sense of Human Scale.
- **Creativity.** Buildings that incorporate innovative and unique design solutions will help define a unique character.
- **Context.** Buildings that are compatible with surrounding buildings and the traditional building hierarchies of the Lower Downtown Historic District, such as the rectilinear and blocky nature of buildings, the block-alley-block pattern on numbered streets, base-middle-top composition, and the extensive use of masonry help ensure harmonious relationships with adjacent neighborhoods.

- **Sustainability.** Buildings that provide opportunities for interaction among a wide variety of people and incorporate environmentally sustainable design approaches will help the district adapt to future urban demands.

PUD-G 19 BUILDING FORM STANDARDS

Chapter 4 of this PUD-G 19 establishes the building form requirements. The standards and guidelines in this section supplement Chapter 4 building form requirements and provide additional standards and guidelines to achieve the guiding principles for building design.

Building Mass & Scale



11. The design standards and guidelines for building mass and scale are intended to encourage building massing that reflects the Lower Downtown character and promotes a strong sense of place through creative and innovative Human Scale design.

INTENT STATEMENTS

- To encourage varied building massing that promotes a sense of place through creative and innovative Human Scale designs
- To promote buildings with cohesive massing and articulation across the Lower Story Façade and Upper Story Façade
- To encourage context-sensitive building massing
- To encourage building mass and scale that is compatible with the traditional building hierarchies of Lower Downtown, including the rectilinear and blocky nature of buildings, the block-alley-block pattern on numbered streets, base-middle-top composition, and generous floor-to-floor heights.

DESIGN STANDARDS

2.01 Buildings shall promote an overall sense of Human Scale.

- Incorporate Upper Story Setbacks to reduce the visual impact of upper stories on the Public Realm (see standards 2.14 - 2.16 for more information)
- Clearly define the Street Level (see standards and guidelines 2.17-2.21 for more information)
- Use materials that convey scale in their proportion, detail and form. Materials applied to provide the appearance of units, panels or modules help to convey a sense of scale (see standards and guidelines 2.38-2.49 for more information).

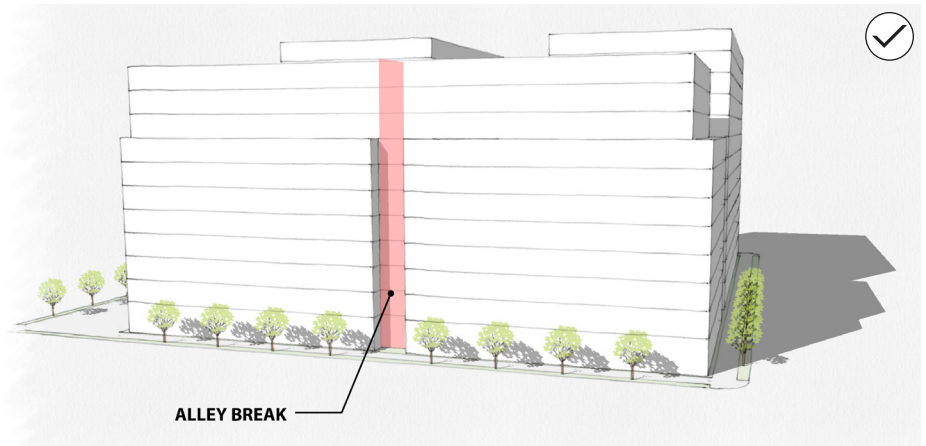
2.02 Buildings shall include a clearly defined base, middle and top, including:

- A pedestrian-oriented Street Level (see standards and guidelines 2.17-2.21 for more information)
- A well-detailed middle with design elements that reflect the building's structural grid, provide depth to the façade, and relate to surrounding buildings
- Architectural rooftop elements such as cornice and cap elements. For a Façade Facing the LoDo

Transition Area, the rooftop element shall be provided at the top of the Lower Story Façade.

2.03 On the Lower Story Façade facing 19th Street, buildings shall have a continuous street wall with changes in plane corresponding to building-alley-building relationships present in Lower Downtown. The plane break shall generally correspond with the location of an alley directly across the street from a building, and shall be at least 20 feet in width and 15 feet in depth.

Building Mass & Scale (Continued)



12. Buildings shall promote an overall sense of Human Scale and should reinforce Lower Downtown's historic fabric.

DESIGN GUIDELINES

2.04 Buildings with over approximately 100 feet of Primary Street frontage should be designed to reduce visual mass and scale and reinforce Lower Downtown's historic fabric.

Appropriate techniques include:

- a. Reducing the appearance of widths along Wazee Street and the West Zone Lot Line using articulation techniques in standards 2.05 and 2.07.
- b. Breaking up the Upper Story Facade massing with Upper Story Setbacks, height variation or separate visual modules.

Building Mass & Scale

Articulation



13. Primary Street-facing Lower and Upper Story Facades shall incorporate vertical & horizontal articulation techniques.

INTENT STATEMENTS

- To promote and reinforce building massing techniques
- To ensure cohesive facade designs
- To promote visually interesting building facades
- To maintain a sense of Human Scale
- To encourage building articulation that is compatible with the traditional building hierarchies of Lower Downtown, including the pattern of longer facades broken by an alley on numbered streets, more varied articulation on named streets, and a consistent bay rhythm.

DESIGN STANDARDS

2.05 A Facade Facing the LoDo

Transition Area shall reflect mass and scale characteristics of the Lower Downtown context.

Appropriate techniques include:

- Horizontal articulation techniques such as cornice lines or belt courses that relate to heights of adjacent buildings in Lower Downtown
- Floor-to-floor heights on the Lower Story Facade above the Street Level of approximately 12' -14'
- Design elements that emphasize the building corners at intersections

2.06 Primary Street-facing Lower and Upper Story Facades, except those defined as a Facade Facing the LoDo Transition Area, shall incorporate vertical & horizontal articulation techniques that reinforce the massing techniques described in standards 2.01-2.03.

Appropriate articulation techniques include:

- Facade plane changes of a minimum of approximately 2-3 inches
- Vertical projections
- Horizontal projections/banding
- Window composition/design

- Balconies or terraces
- Material changes

See Facade Articulation Techniques in Figure 7.3 for more information.

2.07 Articulation techniques, such as a change in materials or setback, shall be used to highlight structural building modules and differentiate building uses at the Street Level.

2.08 Visible Facade areas shall incorporate features to enhance visual interest and avoid the appearance of long blank walls.

Such features include:

- Articulation consistent with design standards 2.06-2.07 or design guideline 2.09
- Transparency consistent with standards for Primary Street-facing Facades
- Wall Murals or other art works

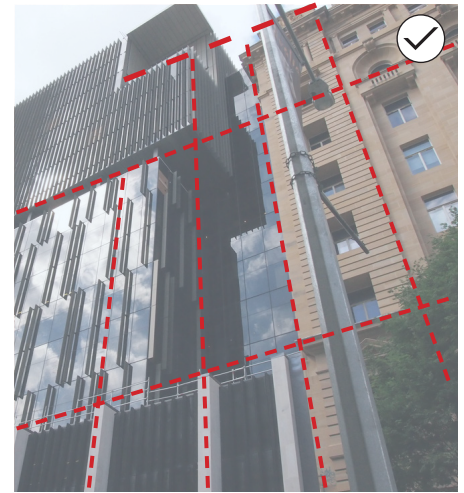
Building Mass & Scale Articulation (continued)



14. Facade articulation should reinforce the tradition of Lower Downtown's historic building fabric.



15. Where balconies and terraces are provided, they should be integrated into vertical and horizontal articulation systems.



16. Horizontal articulation elements should align with the roof lines or articulation on adjacent buildings, when possible.

DESIGN GUIDELINES

2.09 Facade articulation should generally reinforce the tradition of Lower Downtown's historic building fabric.

Appropriate techniques include:

- a. A consistent, articulated bay rhythm
- b. Vertical elements, such as columns, arches, and vertical windows
- c. Delineation of architectural features such as windows, doors, doorways, and courses

See [Relationship of Articulation to Lower Downtown Context](#) below for more information.

2.10 Facade articulation should generally align between the Lower Story Facade and Upper Story Facades to avoid creating a visual disconnection between the building base and upper stories.

2.11 Horizontal articulation elements should align with the roof lines or articulation on surrounding buildings, when possible.

2.12 Where balconies and terraces are provided, they should be integrated into vertical and horizontal articulation systems.

2.13 Balconies should not project past the Lower Story Facade on a Facade Facing the LoDo Transition Area.

RELATIONSHIP OF ARTICULATION TO LOWER DOWNTOWN CONTEXT

Strong features of Lower Downtown's physical character are the rectilinear and blocky nature of its existing buildings and the area's block-alley-block pattern. Numbered streets in Lower Downtown typically have longer facades broken by an alley, and named streets feature more varied articulation.

FACADE ARTICULATION TECHNIQUES

Facade articulation combines with massing techniques to add texture and rhythm that promotes a Human Scaled urban environment with a strong sense of place. The facade articulation techniques described and illustrated below may be used to meet design standard 2.06.

A. FACADE PLANE CHANGES



Facade plane changes include vertical notches or shifts in the building façade to help break down visual mass and scale.

B. VERTICAL PROJECTIONS



Vertical projections include pilasters, columns and other elements that help create a vertical rhythm on the facade.

C. HORIZONTAL PROJECTIONS/BANDING



Horizontal projections and banding include projections, bands, color changes or other elements that help create a horizontal rhythm on the facade.

D. WINDOW COMPOSITION/DESIGN



Window composition and design includes window groupings, use of header/sill elements and window insets or projections that help create a rhythm on the facade.

E. BALCONIES & TERRACES



Inset or cantilevered balconies and terraces help create vertical and horizontal rhythms on the facade that break down building mass and scale into smaller modules.

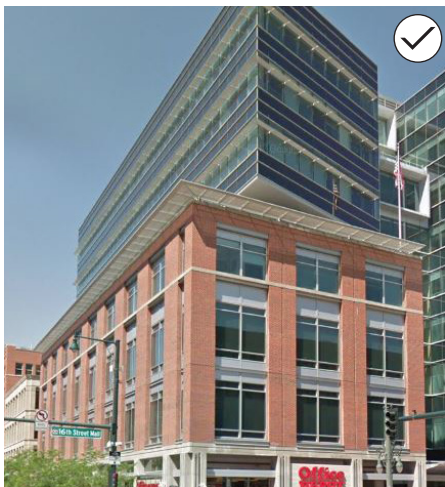
F. MATERIAL CHANGES



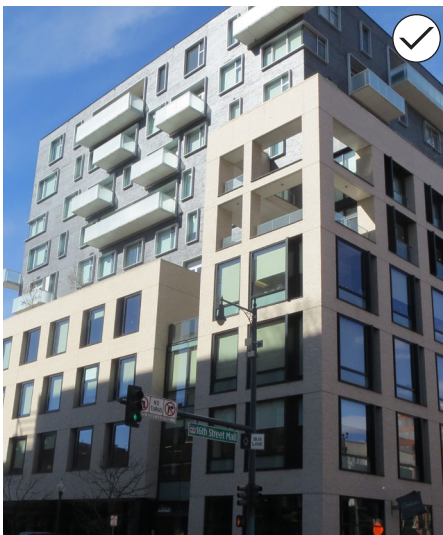
Material changes help create vertical and horizontal rhythms along the facade that break down mass and scale into smaller modules.

Figure 7.3 Facade Articulation Techniques

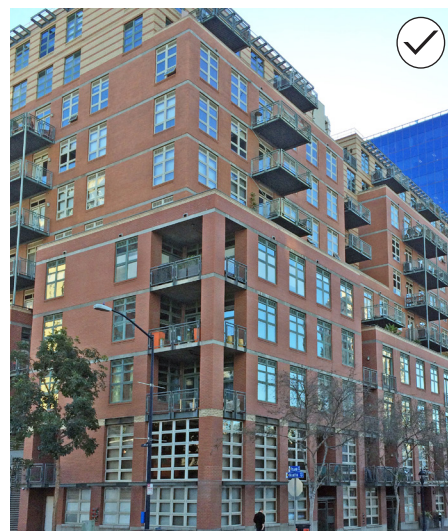
Building Mass & Scale Upper Story Setback



17. Upper Story Setbacks promote facade designs that relate to surrounding buildings.



18. Upper Story Setbacks shall be integrated into the overall building design.



INTENT STATEMENTS

- To encourage varied building massing that promotes a sense of place through creative and innovative Human Scale designs
- To provide compatible building designs that achieve an appropriate transition from Lower Downtown
- To maintain the general appearance of a predominantly 8-story building adjacent to 19th Street, with a distinctive height transition to Upper Story Facades

DESIGN STANDARDS

2.14 Upper Story Setbacks shall be integral to overall building design.

Appropriate techniques include:

- Using Upper Story Setbacks to emphasize building design elements, such as strong corner features
- Integrating a series of Upper Story Setbacks into an overall system of building massing and articulation

2.15 A Facade Facing the LoDo Transition Area shall incorporate design features that provide a distinctive height transition at the location of the Upper Story Setback.

Appropriate techniques include:

- A strong cornice line or similar feature at the top of the Lower Story Facade
- Reinforcing the corners of a building as part of the Lower Story Façade

2.16 In Upper Story Setbacks, buildings shall call themselves out as distinctive from their contextual Lower Story Facade in form, mass and scale. Appropriate techniques include:

- Changes in materials in the Upper Story Facade
- Changes in fenestration in the Upper Story Facade
- Creative massing in the Upper Story Facade

PUD-G 19 UPPER STORY SETBACK REQUIREMENTS

Chapter 4 of this PUD-G 19 requires a minimum Primary Street Upper Story Setback at or below 8 stories and 110 feet on 19th Street and Wazee Street in Subarea A. On 20th Street, along the West Zone Lot Line, and on the remainder of the Wazee Street in Subarea B, Upper Story Setbacks are encouraged but not required.

The guidelines and standards in this section are intended to supplement PUD-G 19 Primary Street Upper Story Setback requirements.

Street Level Design



19. A pedestrian-oriented Street Level shall be clearly defined for each street-facing facade.

INTENT STATEMENTS

- To promote an active pedestrian area with Human Scale design features at the Street Level and adjacent to Open Spaces
- To clearly define a prominent pedestrian area

PUD-G 19 ACTIVE USE REQUIREMENTS

Chapter 4 of this PUD-G 19 requires a minimum percentage of Street Level Active Uses.

The standards and guidelines in this section are intended to supplement Chapter 4 requirements.

DESIGN STANDARDS

2.17 A pedestrian-oriented Street Level shall be clearly defined for each street-facing facade.

Appropriate features to define the Street Level along the Primary Street frontage include:

- a. Awnings and canopies
- b. A prominent cornice above the Street Level
- c. Changes in materials between the Street Level and stories above the Street Level

2.18 The Street Level shall be articulated to promote a Human Scale building frontage.

Appropriate techniques include:

- a. Recessed entries
- b. Changes in Street Level setback

See standards and guidelines 2.01-2.03 and 2.04-2.13 for more information

2.19 The Street Level shall incorporate a substantial floor-to-floor height to promote visual prominence.

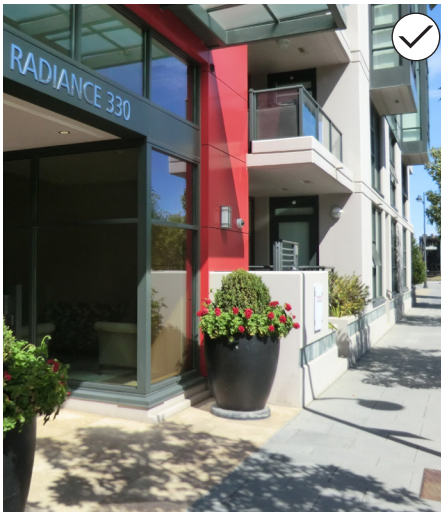
- a. An approximately 12 foot floor-to-floor height minimum is appropriate for a Street Level occupied by residential uses.
- b. An approximately 14 foot floor-to-floor height minimum is appropriate for a Street Level occupied by commercial uses.
- c. Taller Street Level floor-to-floor heights are encouraged.

DESIGN GUIDELINES

2.20 Canopies and awnings used to define the Street Level should be integrated into building design.

2.21 A ground floor facade facing an Open Space that is located on an internal pathway should incorporate articulation and pedestrian-oriented design features and techniques described in 2.17-2.19.

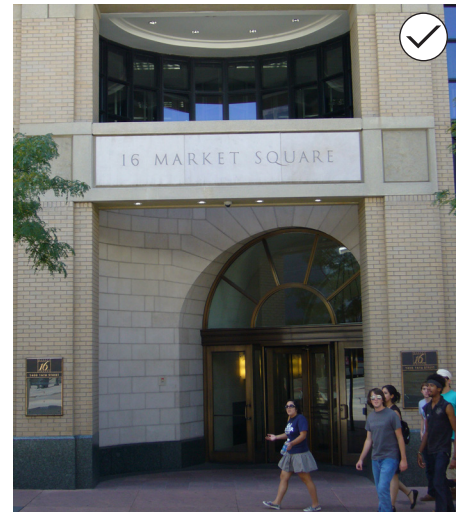
Street Level Design Building Entries



20. Primary pedestrian entrances shall front onto a public street or street-facing Open Space.



21. The design of primary entries shall respond to the Street Level of the building use.



22. Primary pedestrian entrances should be integrated into a signature building element whenever possible.

INTENT STATEMENTS

- To activate the Street Level and integrate pedestrian circulation into building design
- To provide a high number of Street Level entries into active uses to encourage pedestrian activity
- To ensure that pedestrian entries are clearly visible

DESIGN STANDARDS

- 2.22 Primary pedestrian entrances shall front onto a public street or Open Space.
- 2.23 The design of primary entries shall respond to the Street Level building use.
- Locate commercial entrances at the level of the adjacent sidewalk whenever possible.
 - Locate residential entrances no more than approximately 3 feet above the level of the adjacent sidewalk.

DESIGN GUIDELINES

- 2.24 Primary pedestrian entrances should be integrated into a signature building element whenever possible.
- 2.25 Where transit stops are adjacent to a building, a pedestrian entrance should be located adjacent to the stop.
- 2.26 For buildings with multiple tenants, façades should be divided into narrow widths or bays and provide multiple secondary access points to animate the street.

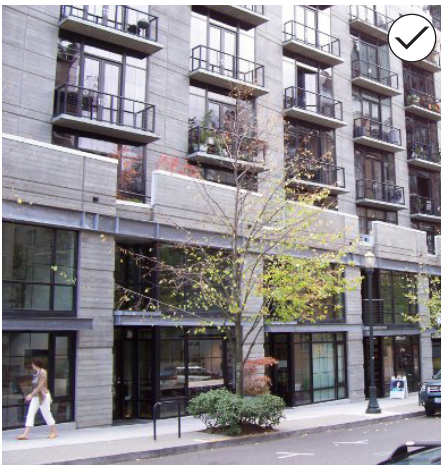
PUD-G 19 PEDESTRIAN ACCESS REQUIREMENTS

Chapter 4 of this PUD-G 19 includes pedestrian access (entrance) requirements to ensure a clear, obvious, publicly accessible connection between the Primary Street and uses within the building.

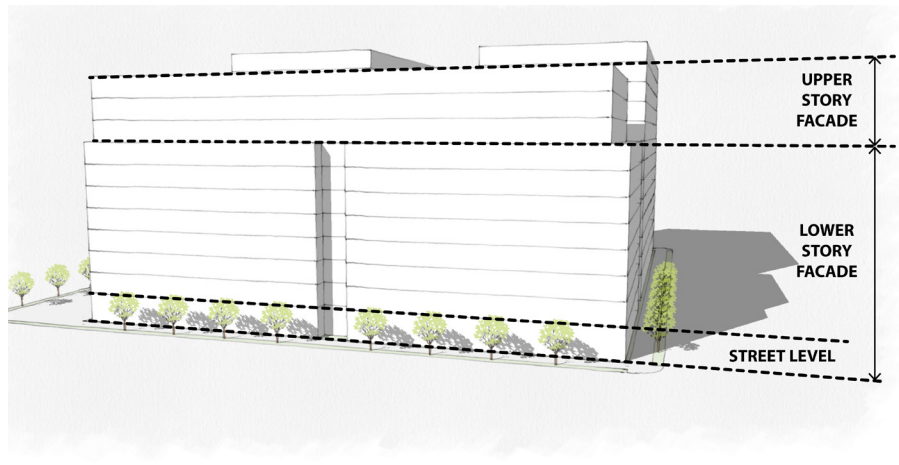
The guidelines and standards in this section are intended to supplement pedestrian access requirements in Chapter 4.

Facade Design

Windows & Transparency



23. The design standards and guidelines for Transparency are intended to encourage visually interesting facade designs that create interplay of light and shadow on the building's surface.



24. Transparency Standards vary for the Street Level, Lower Story Facade and Upper Story Facade.

INTENT STATEMENTS

- To encourage facades that create a sense of Human Scale
- To encourage visually interesting facade designs that create interplay of light and shadow on the building's surface
- To enhance safety with "eyes on the street"
- To ensure that building activities are visible from the Public Realm, including Open Space.
- To ensure that building facades do not cause glare or negative impacts to the Public Realm.

PUD-G 19 TRANSPARENCY REQUIREMENTS

Chapter 4 of this PUD-G 19 requires a minimum percentage of Street Level transparency (the total linear feet of windows or permitted alternatives along the Street Level facade) to provide visual interest, and activate the street and sidewalk.

The standards and guidelines in this section are intended to supplement Chapter 4 Street Level transparency requirements.

DESIGN STANDARDS

- 2.27 Street Level transparent facade areas shall be located to provide visibility into the Street Level Active Uses required by this Chapter 4 of this PUD-G 19.
- 2.28 Street Level windows shall be transparent glass with a maximum reflectance of approximately 0.15 that allows for pedestrians to view the activity within the building.
- 2.29 Lower Story Facades, excluding the Street Level, shall incorporate a minimum of 50% transparent glass with a maximum reflectance of approximately 0.30.
- 2.30 Upper Story Facades shall incorporate a minimum of 40% transparent glass with a maximum reflectance of approximately 0.35.
 - a. Use of opaque/spandrel glass is acceptable to continue glazing patterns where screening of utilities or structural elements is required.
- 2.31 Exterior reflective coatings shall not be used on transparent window glazing.

TRANSPARENCY FOR STRUCTURED PARKING

The standards and guidelines in this Transparency section do not apply to Visible Structured Parking. See "Structured Parking Design" for standards and guidelines that promote openings in a structured parking facade to produce the impression of transparent facade areas.

Facade Design

Windows & Transparency (continued)



25. The design standards and guidelines encourage visually interesting facade designs that create interplay of light and shadow on the building's surface.

DESIGN STANDARDS (Continued)

2.32 Window openings shall be designed to provide depth of detail on the facade.*

Appropriate techniques include:

- a. Recessing the face of glass a minimum of 4 to 6 inches behind the facade

*This standard shall not apply to a glass curtain wall.

DESIGN GUIDELINES

2.33 A ground floor facade facing an Open Space that is located on an internal pathway should incorporate transparent windows linked to building uses. Transparency reflectance should be consistent with standard 2.28-2.30.

2.34 For mixed-use developments, levels of transparency should reflect different uses within the building.

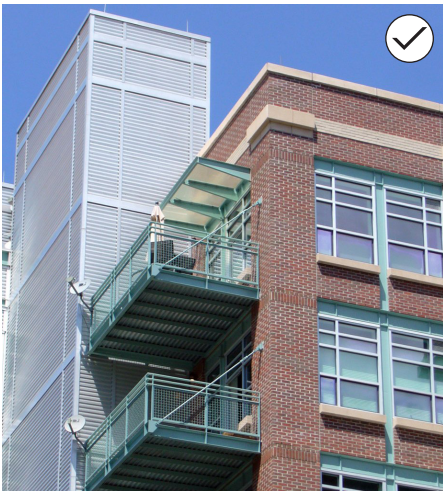
- a. A lower glass-to-wall ratio is typical of residential uses.
- b. A higher glass-to-wall ratio is typical of commercial uses.

2.35 Balcony railings should not significantly block visibility of facade areas used to meet transparency standards.

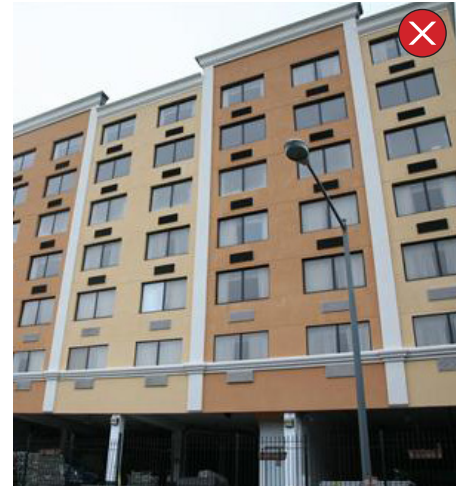
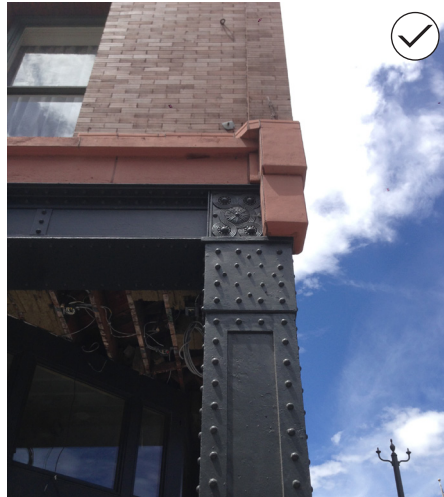
2.36 Clear, "Low E," or slightly tinted windows should be used to ensure the visibility of pedestrian-oriented commercial uses.

2.37 Landscaping should not significantly block transparent window areas at the Street Level.

Facade Design Building Materials



26. The use of durable building materials and material treatments should promote the sense of Human Scale.



27. EIFS and other synthetic stucco materials shall not be used.

INTENT STATEMENTS

- To ensure that building materials used on any Visible Facade provide visual interest and a sense of Human Scale
- To discourage building materials that produce flat or featureless surfaces on any Visible Facade
- To encourage the use of innovative, high-quality and sustainable materials
- To promote use of durable building materials and material treatments
- To ensure that building materials are integrated into a cohesive facade design
- To provide a compatible response to materials present in Lower Downtown, including brick, stone, terra-cotta tile, and ceramic tile

DESIGN STANDARDS

- 2.38 Building materials used on a Primary Street-facing Facade shall be of proven durability.
- a. Applicants may be required to demonstrate the durability of unproven or unusual materials.
- 2.39 Building materials used on a Primary Street-facing Facade shall be properly finished and detailed.

- 2.40 Any change in materials shall be combined with a variation in the wall plane.
- 2.41 A Facade Facing the LoDo Transition Area or facing Wazee Street shall incorporate a minimum of 60%* Masonry on the Lower Story Facade.
- Appropriate Masonry materials include:
- a. Brick
 - b. Stone and Terra cotta
- *Not including window, transom glass and door areas.
- 2.42 On a Facade Facing the LoDo Transition Area or on a facade facing Wazee Street, Cementitious Stucco shall not be used on more than 20% of the Lower Story Facade.
- 2.43 On a facade that is not defined as a Facade Facing the LoDo Transition Area and is not facing Wazee Street, Cementitious Stucco shall not be used on more than 50% of the Lower Story Facade.

- 2.44 Fiber Cement Siding materials shall not be used on more than 50% of a facade defined as the Primary Street-facing Facade.
- 2.45 EIFS (Exterior Insulating Finish Systems) and other synthetic stucco materials shall not be used.

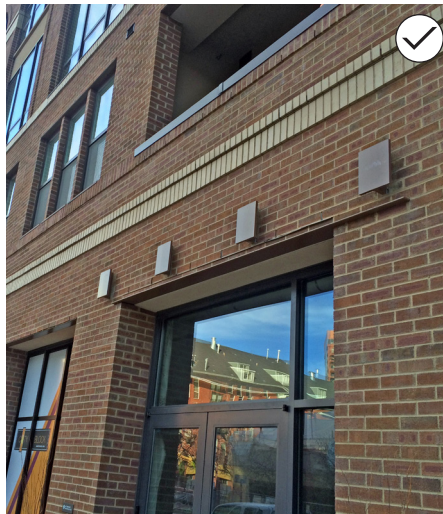
DESIGN GUIDELINES

- 2.46 Building materials should be selected and applied to convey a sense of Human Scale.
- Appropriate techniques include:
- a. Adding visual interest through texture, finish and detailing
 - b. Applying materials in units, panels or modules that produce shadow lines to help convey a sense of scale
- 2.47 Any change in building materials should occur at the inside corner of a variation in the wall plane.
- 2.48 Carefully detailed materials should be used to reinforce building mass, scale and articulation techniques.
- 2.49 Cementitious Stucco should not be used on Upper Story Facade areas that may be difficult to reach or maintain.

Facade Design Designing for Signage



28. Whenever possible, use overall building design or architectural features to market building tenants. Appropriate strategies include using iconic building elements to emphasize storefronts or tenant entries.



29. Facade designs should integrate power sources for future signage lighting, whenever possible.



30. Facade designs shall consider potential future locations for pedestrian-oriented signage.

INTENT STATEMENTS

- To ensure that facade designs consider potential future locations for pedestrian oriented signage
- To encourage facade designs that promote harmonious relationships between overall building mass and scale, architectural features and potential future signage

PUD-G 19 SIGN REQUIREMENTS

As stated in Chapter 6 of this PUD-G 19, Signs in PUD-G 19 must comply with Denver Zoning Code Article 10 regulations as applicable to the D-AS-12+ zone district.

The guidelines and standards in this section are intended to supplement sign requirements in Chapter 6.

DESIGN STANDARDS

2.50 Facade designs shall consider potential future locations for pedestrian-oriented signage.

- Appropriate strategies include:
- Incorporating a designated band or area for signage above the Street Level for potential future signage
 - Designing canopies and awnings to accommodate potential future wall signage
 - Designating areas to accommodate tenant or directory signage near primary building entries

2.51 Where applicable, facade designs shall consider locations for potential large-scale building identification signage.

- Appropriate strategies include:
- Limiting large-scale building identification signage to taller buildings (approx. 12+ stories)
 - Reserving an area along the roof parapet, or integrated into a roof cap feature, for future large-scale building identification signage

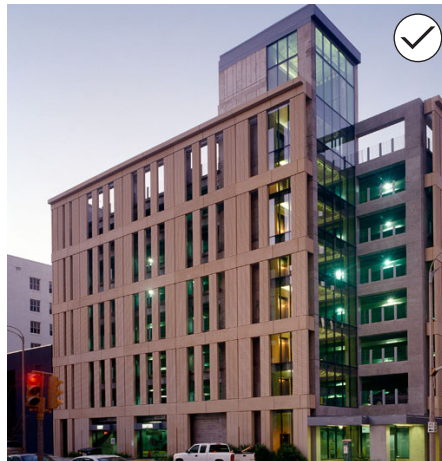
DESIGN GUIDELINES

2.52 Overall building design or architectural features should be used to market building tenants rather than signage, whenever possible.

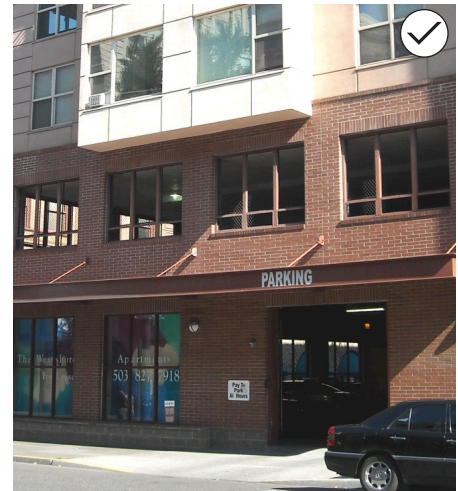
- Appropriate strategies include:
- Using building massing techniques to create projecting or recessed facade elements that emphasize storefronts or tenant entries
 - Locating architectural features to highlight storefronts or tenant entries
 - Using iconic building elements to emphasize storefronts or tenant entries
 - Incorporating building address signage into the design of the facade

2.53 Facade designs should integrate power sources for future signage lighting, whenever possible.

Structured Parking Design



31. The design standards and guidelines promote structured parking designs that are compatible with the character and quality of the overall building facade and adjacent buildings.



32. Facade areas with Visible Structured Parking shall reflect the overall pattern of openings on the building facade.

INTENT STATEMENTS

- To promote structured parking designs that are compatible with the character and quality of the overall building facade and adjacent building facades
- To minimize visual and physical impacts of parked cars on the pedestrian experience

PUD-G 19 LIMITATION ON STRUCTURED PARKING VISIBILITY

Chapter 4 of this PUD-G 19 requires that visibility of structured parking is limited.

The standards and guidelines in this section supplement Chapter 4 standards to address the design of any Visible Structured Parking.

Note that PUD-G 19 does not require a minimum number of on-site parking spaces.

DESIGN STANDARDS

2.54 Where provided, Visible Structured Parking shall be integrated into overall facade design.*

Appropriate techniques include:

- Continuing similar building materials across facade areas with Visible Structured Parking
- Continuing vertical and horizontal articulation across facade areas with Visible Structured Parking

2.55 Facade areas with Visible Structured Parking shall reflect the overall pattern of openings on the building facade.*

- Use similar opening proportions to those on the overall facade.

2.56 Align openings with those on adjacent buildings or facade areas.

2.57 Facade areas with Visible Structured Parking shall incorporate openings that reflect transparency standards for non-parking facades.

2.58 Facade areas with Visible Structured Parking shall be designed to limit the view of parked cars and angled ramps from the Public Realm

2.59 Facade areas with Visible Structured Parking shall be designed to minimize the visual

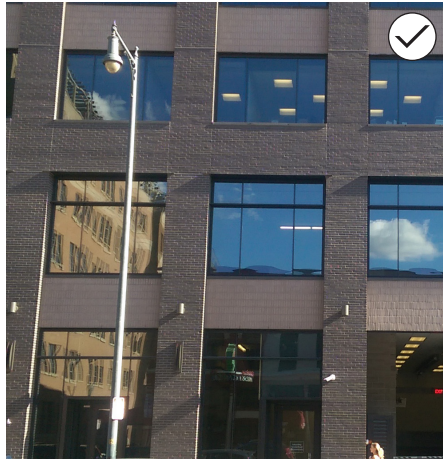
impacts of security lighting and headlights.

Appropriate techniques include:

- Use of non-transparent materials for approximately the first 36 to 48 inches of the facade to block the view of headlights
- Architectural features that block the view of ceiling and security lighting
- Use of fully-shielded LED or other lighting not exceeding approximately 6,500 lumens

**Standards 2.54 and 2.55 are not applicable to stand-alone parking structures that do not include non-parking uses on upper floors.*

Structured Parking Design (continued)



33. The facade of structured parking that is fully enclosed and ventilated should incorporate materials and finishes similar to those used on the overall facade.

DESIGN GUIDELINES

- 2.60 Street Level facade areas with Visible Structured Parking should be designed to accommodate future conversion to non-parking uses.
- 2.61 Ramped portions of structured parking should not be located on a Primary Street-facing Facade unless they are wrapped with another use.
- 2.62 Facade areas with Visible Structured Parking should be fully enclosed and ventilated whenever possible.
- 2.63 Dedicated parking spaces should be provided for car share services, whenever possible.

ARTISTIC SCREENING OF A PARKING STRUCTURE



Artistic screens may sometimes be appropriate for facade areas with Visible Structured Parking. While such screens may vary in character from the overall building facade and adjacent building facades, they may be approved on a case-by-case basis where they:

- » Are a “work of public art” as defined by Section 20-86 of the Denver Revised Municipal Code, as determined by the Zoning Administrator with input from Denver Arts and Venues
- » Limit the view of all parked cars and angled ramps from adjacent plazas, public rights-of-way, private streets and plazas or Open Space

Chapter 7. Glossary of Terms

The terms included here are defined solely for Chapter 7, Standards and Guidelines, of PUD-G 19. For terms that are not included here, refer to PUD-G 19 and the [Denver Zoning Code](#), Section 13.3 Definition of Words, Terms and Phrases as well as Section 11.12 Use Definitions.

ACTIVE USES

See [Street Level Active Uses](#) and [Highly Active Uses](#)

AMENITY ZONE

An area between the street and sidewalk that is improved with street trees, paving, street furniture or other amenities. An [Amenity Zone](#) is typically located within the [Public Right-of-Way](#).

BICYCLE CORRAL

Bicycle Corrals are an on-street bicycle parking facility that can accommodate bicycles in the same area as an on street- vehicle parking space. They work best where sidewalks are too narrow to accommodate bicycle racks and in areas with both high levels of people bicycling and demand for bicycle parking.

CEMENTITIOUS STUCCO

An exterior wall finish, usually composed of cement, sand, and lime, and applied while wet.

BICYCLE FACILITY

A lane, path, or shoulder for use by bicyclists, or a shelter/ parking facility for bicycles.

ENHANCED SETBACK

An [Enhanced Setback](#) is the space created when buildings are set back from the Primary Street Zone Lot Line, but generally still positioned within the Primary Street build-to range provided in PUD-G 19. Such setbacks should be enhanced to provide attractive and usable areas that activate the street frontage. They can range in size from modest setback areas provided by building offsets to larger patio seating or pedestrian use areas.

FACADE FACING THE LODO TRANSITION AREA

A façade facing 19th Street or Wazee Street in Subarea A defined in PUD-G 19.

FIBER CEMENT SIDING

A building material used to cover the exterior of a building. Fiber cement is a composite material made of sand, cement and cellulose fibers. Fiber Cement Siding includes HardieBoard and HardiePlank.

HIGHLY ACTIVE USES

Uses that contribute to the activation and engagement of the pedestrian experience. These uses include (but are not limited to): retail storefronts; restaurants and cafes; building lobbies and amenity areas; indoor art or recreation facilities; arts and cultural facilities. Uses that are not considered Highly Active Uses are residential units, light warehousing, mini-storage, parking spaces or parking aisles. Also see [Street Level Active Uses](#).

HUMAN SCALE

A sense of [Human Scale](#) is achieved when one can reasonably interpret the size of a building by comparing features of its design to comparable elements in one's experience.

LOW IMPACT DEVELOPMENT

Low Impact Development (LID) is stormwater management approach to address rainfall in a way which more closely mimics the natural hydrologic system at the site prior to any development.

LOWER STORY FACADE

The [Lower Story Facade](#) will generally be considered as stories 1-8 (up to 110 feet, including the [Street Level](#)) of the [Primary Street-Facing Facade](#), with everything above defined as the [Upper Story Facade](#). However, where [Upper Story Setbacks](#) are located below the eighth story, the [Lower Story Facade](#) may be considered as stories below the [Upper Story Setback](#) with everything above the setback defined as the [Upper Story Facade](#).

MASONRY

Building materials characterized by individual units laid in and bound together by mortar. [Masonry](#) materials include brick, stone and terra cotta.

OPEN SPACE

An [Open Space](#) is a type of [Enhanced Setback](#) that may extend beyond the Primary Street build-to range allowed by PUD-G 19 to provide more substantial [Street Level](#) pedestrian use areas, such as: courtyards, plazas, pocket parks, and pedestrian pathways leading into a development. A [Private Open Space](#) required in PUD-G 19 is also an [Open Space](#). See also [Private Open Space](#).

Glossary of Terms (continued)

PRIMARY STREET

A Primary Street Zone Lot Line as defined in PUD-G 19. PUD-G 19 defines 19th Street, Wazee Street, and the West Zone Lot Line as Primary Street Zone Lot Lines.

PRIMARY STREET-FACING FACADE

Any facade facing a Primary Street Zone Lot Line defined in PUD-G 19.

PRIMARY STREET UPPER STORY SETBACK

See [Upper Story Setback](#)

PRIVATE OPEN SPACE

An [Open Space](#) located on private property that is a required design element in PUD-G 19 and/or used as a build-to alternative in PUD-G 19.

PUBLIC REALM

Areas within the [Public Right-of-Way](#) (including streets and sidewalks), as well as parks, [Enhanced Setbacks](#), [Open Space](#) and connecting paths or walkways located on private property adjacent to the [Public Right-of-Way](#).

PUBLIC RIGHT-OF-WAY

The area of land owned by the municipality over which the road and sidewalk is built. The [Public Right-of-Way](#) generally includes the roadway, sidewalks, amenity zone and alley areas.

QUALITY

Refers to the use of a material that is low maintenance, will stand up to wear and tear and is appropriate for the intended use or design application.

RIGHT-OF-WAY

See [Public Right-of-Way](#)

SCALE

The perception of a building's size in relation to a human, based on proportions, scaling elements and contextually sensitive solutions to the design of the structure. Scale is one of the elements that contribute to our perception of a place's character.

STREET LEVEL

The first story or level in a building or structure, as defined by the [Denver Zoning Code](#). Note that the [Street Level](#) is considered to be part of the [Lower Story Facade](#).

STREET LEVEL ACTIVE USES

Uses that contribute to the activation and engagement of the street, as defined by Chapter 4 of this PUD-G 19. Chapter 4 PUD-G 19 requires a minimum percentage of [Street Level Active Uses](#) on a [Primary Street-facing Facade](#). Also see [Highly Active Uses](#).

UPPER STORY FACADE

The [Upper Story Facade](#) will generally be considered as the [Primary Street-facing Facade](#) on all stories above the eighth story (110 feet). However, where [Upper Story Setbacks](#) are located below the eighth story, the [Upper Story Facade](#) may begin above the [Upper Story Setbacks](#).

UPPER STORY SETBACK

A PUD-G 19-required building setback at a maximum specified height above the [Street Level](#) to provide appropriate pedestrian height, scale and massing. May also refer to other setbacks above the eighth story as described in these standards and guidelines.

VISIBLE FACADE

Any facade that is not a [Primary Street-facing Facade](#) but is visible from the [Public Realm](#) at the time of construction without significant blockage by building or site features.

VISIBLE STRUCTURED PARKING

[Visible Structured Parking](#) refers to a structured parking facade that is not wrapped with another use and is located on the [Primary Street-facing Facade](#), or is visible from the [Public Realm](#) at the time of construction without significant blockage by building or site features.

WALL MURAL

A mural is any piece of artwork or super graphic (which does not serve as an advertisement and is not a sign) painted or applied directly on a wall.

WYNKOOP PROMENADE

A publicly accessible pedestrian walkway required by PUD-G 19 that connects the 19th Street right-of-way to the 20th Street right-of-way.

CHAPTER 8. RULES OF INTERPRETATION

Subject to Chapter 9 of this PUD-G 19, whenever a section of the Denver Zoning Code is referred to in this PUD-G 19, that reference shall extend and apply to the section referred to as subsequently amended, recodified, or renumbered; provided, however, if a section of the Denver Zoning Code, as subsequently amended, recodified, or renumbered conflicts with a provision of this PUD-G 19, this PUD-G 19 shall control.

CHAPTER 9. VESTED RIGHTS

This PUD-G 19 shall be established in accordance with Denver Zoning Code Sections 9.6.1.2.C and 9.6.1.5, and vested property rights shall be created 90 days after the effective date of the ordinance approving this PUD-G 19. The property rights vested through approval of this PUD-G 19 shall remain vested for a period of 3 years and shall include the right to commence and complete development of and the right to use the site in accordance with the intent, standards, and uses set forth in the Denver Zoning Code, as amended from time to time, except as expressly modified by this PUD-G 19.